

| Application Number | Date of Appln | Committee Date | Ward |
|---------------------------|----------------------|-----------------------|---------------|
| 124995/FO/2019 | 1st Oct 2019 | 19th Dec 2019 | Cheetham Ward |

Proposal Erection of a five storey educational building (18495 sqm) (Use Class D1) with associated access, servicing, landscaping, public realm and other associated works following removal of existing car park

Location Land on the Corner of Great Ducie Street and New Bridge Street, Manchester, M3 1WB

Applicant LTE Group, C/o Agent

Agent Mr Ed Harvey, Avison Young, Norfolk House, 7 Norfolk Street, Manchester, M2 1DW

Description

This 0.88 hectares site is used as a surface car park and is bounded by Great Ducie Street, New Bridge Street and existing surface car parking. It is roughly rectangular in shape and slopes towards Great Ducie Street. In the south eastern part of the site the topography rises more steeply towards New Bridge Street.

The surrounding area contains a mixture of light industrial buildings, warehouses and surface level car parks. There is a Travelodge hotel to the north of the site and a residential development to the south west. The Manchester Arena is opposite the site on New Bridge Street. Srangeways Prison is to the north east. The site is part of the Boddingtons Strategic Regeneration Framework. Victoria Station is directly opposite the site providing access to trains, trams and bus services across Manchester and beyond.

Proposal

The application proposes a 5 storey building to create a new college (use class D1) with classrooms, office accommodation and other facilities. The building would become the new 'City Centre Campus' for Manchester College and UCEN Manchester as a result of consolidation of various other education campuses and buildings across the city into this one location. The College would have a particular focus on Creative and Performing Arts providing a new theatre space, film studios, TV studios, beauty salon, photography studio, arts studio, restaurant and kitchen (including bakery), learning resource centre, music practice rooms and general teaching spaces.

The design and layout is centred around a theatre box located in an internal courtyard. The courtyard would be used as a social space for the college as well as public events and theatre nights.

There would be an entrance for UCEN on New Bridge Street and one for Manchester College on Great Ducie Street to ensure that they retain their distinctive

identities but can share facilities. Internally the building would be 'zoned' with the UCEN facilities located in the eastern and southern parts of the building and with the college in the northern and western parts.



Ground floor layout showing UCEN and Manchester College entrances

The ground floor contains a courtyard atrium, theatre, film studios, music rehearsal and recording studios, bands rooms, TV studios, DJ classrooms and student hub as well as restaurant and kitchen facilities.

At the first floor, a staircase would link the ground and first floor courtyard spaces and provide access to the main dining space shared by all students. There would be a library and learning resource centre, a business and computing studio and classrooms and edit facilities for the film school at this level.

At the second floor there would be computing and media provision. Further learning spaces associated with the library and learning and resource centre would be located on this level which will include a variety of different learning spaces.

The third floor would be occupied by the visual arts and include specialist workshops and studios. Flexible art studio spaces would provide space for shows and exhibitions. The hair and beauty curriculum would be delivered from this floor with beauty treatment rooms, specialist areas for media and special effects makeup would be provided on this floor.

The fourth floor would be dedicated to the specialist performing arts rehearsal facilities for both UCEN and Manchester College. Access would be provided to an external roof terrace for both UCEN and Manchester College students.

The building would consist of a dark brick and glazing. Full height vertical windows, with coloured metallic panels, provide a distinctive pattern across the façade. Double height glazed curtain walling would highlight the entrances to the building and animate the streets.



Great Ducie Street entrance

The internal courtyard would be expressed by a regular timber grid contrasting with the dark external façade. Open gallery walkways, glazed screens and timber panels would provide a rich internal elevation to the building.



Internal courtyard including theatre box

A glazed roof would cover the atrium. The theatre box would be finished with folded metal panels in a pyramidal shape. New public realm would provide safe spaces for students external to the main building. New street trees will be provided along Great Ducie Street and New Bridge Street.

An access road at the northern end of the site would connect the landscape area along Great Ducie Street with the Old Brewery Gardens landscape area ensuring permeability.

An area to the rear would eventually form a second phase and would be landscaped to provide an external social space for students and 112 cycle parking, refuse storage, service bay and six accessible parking spaces (plus a further two disabled spaces within a lay by (8 disabled bays in total)).

Consultations

Local residents/public opinion

The proposal has been advertised as a major development, being of public interest together with affecting the setting of a conservation area and listed buildings and being a proposal subject to an Environmental Impact Assessment. Site notices were displayed at the site. In addition, notification letters have been sent to an extensive area, local residents and businesses.

Two objections can be summarised as follows:

- The principle is welcomed but the low density of the proposal is not supported. The Boddingtons SRF envisaged and 11/30plus building on this site. To build only 5 storeys would be a waste of a large site next to a major

transport interchange. The proposal should provide for the college as well as residential or office accommodation above. There should be an impressive building on this site at this road junction;

- There is a lack of ground floor commercial units, particularly along Great Ducie Street. The Great Ducie Street SRF envisages this as a new thoroughfare and its vital to encourage residents into this new area;
- This proposal would result in the overdevelopment of the site which will increase criminal activities in the area (there is over 1900 recorded criminal offences in this area);
- The proposed building will attract environmental issues such as light pollution, noise and increase carbon footprint;
- There will be highway safety issues during the construction work and increased vehicle movements upon completion of the building.

Highway Services

No objection subject to the provision of a robust travel plan, appropriate cycle storage, provision of disabled parking spaces, operational plan for the access road and provision of a crossing along Great Ducie Street and Trinity Way. A construction management plan should be agreed.

Environmental Health

The waste management strategy is acceptable. Further details are required in respect of the acoustic insulation together with details of any plant. In addition, fume extraction and ground conditions mitigation are required to be agreed along with clean air inlets.

Flood Risk Management Team

The drainage strategy for the site should be agreed. A verification report shall be submitted on completion of the works.

Design for Security at Greater Manchester Police

The Crime Impact Statement submitted with the application should be implemented.

Greater Manchester Ecology Unit

There are no ecology constraints and biodiversity improvements should be implemented along with the protection measures.

Greater Manchester Archaeology Unit

There could be remains of early 19th to early 20th century buildings on the site together with the 18th century Brewery. Further site investigations should be undertaken and secured by planning condition.

Salford City Council

No objections to the proposal

Policy

The Development Plan

The Development Plan consists of:

- The Manchester Core Strategy (2012); and
- Saved policies of the Unitary Development Plan for the City of Manchester (1995)

The Core Strategy Development Plan Document 2012 -2027 is the key document in Manchester's Local Development Framework. It sets out the long term strategic planning policies for Manchester's future development.

A number of UDP policies have been saved until replaced by further development plan documents to accompany the Core Strategy. Planning applications in Manchester must be decided in accordance with the Core Strategy, saved UDP policies and other Local Development Documents as directed by the National Planning Policy Framework (NPPF).

The NPPF requires application to be determined in accordance with the Development Plan unless material considerations indicate otherwise.

Manchester Core Strategy Development Plan Document (July 2012)

The relevant policies within the Core Strategy are as follows:

SO1. Spatial Principles – This is a strategic site within the Boddingtons Brewery SRF. The proposal would provide a state of the art educational building adjacent to other commercial and residential developments and public transport. The development would support economic growth and job creation by offering a high quality educational building within a highly sustainable location.

SO2. Economy – High quality educational accommodation in a sustainable location such as this, would support the economic growth of the city. The development would support local employment during the construction phases as well as offering the most up to date facilities as part of the overall educational offer.

S06. Environment – The development would be low carbon and highly sustainable using up to date energy efficiency measures in the fabric and construction of the building. Roof gardens would have air quality, biodiversity and drainage benefits. The development is supported by a robust travel plan and 112 cycle spaces and no on site car parking other than six disabled spaces (plus to additional flexible disabled bays).

Policy SP1 'Spatial Principles – The proposal would have a positive impact on economic growth and education provision in a highly sustainable location. The

design and appearance of the building would provide a high quality addition to the street scene and provide social and arts space for students and the general public.

Policy EC3 'The Regional Centre', Primary Economic Development Focus (City Centre and Fringe and Policy CC8 Change and Renewal – The proposal would provide a modern education building close to all forms of sustainable transport.

Policy CC5 Transport – The site is highly sustainable next to Manchester Victoria Train station where students and staff will have access to rail, tram and bus services. Over 112 secure cycle spaces are proposed to support alternative forms of travel which will be encouraged through a travel plan.

Policy CC8 Change and Renewal – The proposal would provide a high quality educational facility on a previously developed city centre site. The proposal would support economic growth as well as social and environmental objectives.

Policy CC9 Design and Heritage – The proposal provides a high quality building on a strategic city centre site.

Policy CC10 A Place for Everyone – The proposal would complement the ongoing regeneration of the former Boddingtons Brewery site and have six accessible car parking spaces. All internal and external spaces would be fully accessible.

Policy T1 'Sustainable Transport' - The site has access to a range of public transport modes.

Policy T2 'Accessible areas of opportunity and needs' - A transport assessment and travel plan demonstrates that the proposal would have minimal impact on the local highway network and would encourage the use of sustainable transport.

Policy EC1 Employment and economic growth in Manchester – The proposal would provide educational floor space specialising in the creative arts. The social and theatre accommodation would be in a sustainable location.

Policy EN1 'Design principles and strategic character areas' - This high quality scheme would enhance the regeneration of the area.

Policy EN3 'Heritage' - The impact on the historic environment would be acceptable and this is considered in further detail within the report.

EN4 'Reducing CO₂ emissions by enabling low and zero carbon development' –The building fabric would be energy efficient. It would have no car parking apart from six bays for disabled people and a travel plan and cycle provision. The proposal includes renewable technologies to ensure energy demands are sustainable and low carbon.

Policy EN5 'Strategic areas for low and zero carbon decentralised energy infrastructure' - The building would be energy efficient and travel planning would promote sustainable travel patterns.

Policy EN6 'Target framework for CO₂ reductions from low or zero carbon energy supplies' - The buildings functions would reduce overall energy demands. The building fabric is high quality and energy costs should remain low. Renewable energy would ensure sustainable energy is used.

Policy EN9 'Green Infrastructure' – The green infrastructure would provide biodiversity improvements as well as new habitats through roof gardens, street trees and external spaces.

Policy EN14 'Flood Risk'- A scheme to minimise surface water runoff would be agreed and be in line with the sequential approach, and mitigation as a result of the sites location in flood zone two.

Policy EN15, 'Biodiversity and Geological Conservation' - The site has limited ecological value and the planting proposed would represent a significant biodiversity enhancement. No clearance of the limited vegetation at the site should take place during bird nesting season.

Policy EN16 'Air Quality' The proposal would replace an existing car park and would not compromise air quality. There is limited car parking with the use and scheme has been designed to protect users inside the building from local air quality.

Policy EN17 'Water Quality' - The proposal includes water saving measures and would minimise surface water runoff.

Policy EN18, 'Contaminated Land' – The ground conditions are not complex and can be adequately dealt with.

EN19 'Waste' – Recycling principles are incorporated in the waste management strategy.

Policy DM1 'Development Management' - Careful consideration has been given to the design, scale and layout of the building.

For the reasons given below, it is considered that the proposal is consistent with the policies contained within the Core Strategy.

The Unitary Development Plan for the City of Manchester (1995)

The Unitary Development Plan for the City of Manchester was adopted in 1995. However, it has now been largely replaced by the Manchester Core Strategy. There are some saved policies which are considered relevant and material and therefore have been given due weight in the consideration of this planning application. The relevant policies are as follows:

Saved policy DC26, Development and Noise - The impact from noise sources would be minimised and further mitigation would be secured by planning condition.

Saved policy DC18 Conservation Areas – It is considered that the proposal would maintain the character and appearance of the adjacent conservation area. This is discussed in more detail later in the report.

Saved policy DC19 ‘Listed Buildings’ - The proposal would not affect the setting of nearby listed buildings or conservation areas.

Saved policy DC20 Archaeology – An archaeological desk based assessment has been carried out for the site and it is considered that the development would not have an impact on any potentially significant remains on the site.

Saved policy E3.3- The proposal will provide a high quality building along New Bridge Street and would enhance the appearance of this main radial route.

For the reasons given below, it is considered that the proposal is consistent with the policies contained within the UDP.

Other material policy considerations

The Guide to Development in Manchester Supplementary Planning Document and Planning Guidance (Adopted 2007)

This document provides guidance to help develop and enhance Manchester. In particular, the SPD seeks appropriate design, quality of public realm, facilities for disabled people (in accordance with Design for Access 2), pedestrians and cyclists. It also promotes a safer environment through Secured by Design principles, appropriate waste management measures and environmental sustainability. Sections of relevance are:

- Chapter 2 ‘Design’ – outlines the City Council’s expectations that all new developments should have a high standard of design making a positive contribution to the City’s environment;
- Paragraph 2.7 states that encouragement for “the most appropriate form of development to enliven neighbourhoods and sustain local facilities. The layout of the scheme and the design, scale, massing and orientation of its buildings should achieve a unified form which blends in with, and links to, adjacent areas.
- Paragraph 2.8 suggests that in areas of significant change or regeneration, the future role of the area will determine the character and design of both new development and open spaces. It will be important to ensure that the development of new buildings and surrounding landscape relates well to, and helps to enhance, areas that are likely to be retained and contribute to the creation of a positive identity.
- Paragraph 2.14 advises that new development should have an appropriate height having regard to the location, character of the area and specific site circumstances. Although a street can successfully accommodate buildings of differing heights, extremes should be avoided

unless they provide landmarks of the highest quality and are in appropriate locations.

- Paragraph 2.17 states that vistas enable people to locate key buildings and to move confidently between different parts of the neighbourhood or from one area to another. The primary face of buildings should lead the eye along important vistas. Views to important buildings, spaces and landmarks, should be promoted in new developments and enhanced by alterations to existing buildings where the opportunity arises.

– Chapter 8 ‘Community Safety and Crime Prevention’ – The aim of this chapter is to ensure that developments design out crime and adopt the standards of Secured by Design;

– Chapter 11 ‘The City’s Character Areas’ – the aim of this chapter is to ensure that new developments fit comfortably into, and enhance the character of an area of the City, particularly adding to and enhancing the sense of place.

Manchester Green and Blue Infrastructure Strategy 2015

The Manchester Green and Blue Infrastructure Strategy (G&BIS) sets out objectives for environmental improvements within the City in relation to key objectives for growth and development.

Building on the investment to date in the city's green infrastructure and the understanding of its importance in helping to create a successful city, the vision for green and blue infrastructure in Manchester over the next 10 years is:

By 2025 high quality, well maintained green and blue spaces will be an integral part of all neighbourhoods. The city's communities will be living healthy, fulfilled lives, enjoying access to parks and greenspaces and safe green routes for walking, cycling and exercise throughout the city. Businesses will be investing in areas with a high environmental quality and attractive surroundings, enjoying access to a healthy, talented workforce. New funding models will be in place, ensuring progress achieved by 2025 can be sustained and provide the platform for ongoing investment in the years to follow.

Four objectives have been established to enable the vision to be achieved:

1. Improve the quality and function of existing green and blue infrastructure, to maximise the benefits it delivers
2. Use appropriate green and blue infrastructure as a key component of new developments to help create successful neighbourhoods and support the city's growth
3. Improve connectivity and accessibility to green and blue infrastructure within the city and beyond
4. Improve and promote a wider understanding and awareness of the benefits that green and blue infrastructure provides to residents, the economy and the local environment.

Former Boddingtons Brewery Site Strategic Regeneration Framework (SRF) (2015)

The SRF was adopted by the City Council's Executive Committee in November 2015. The documents purpose is to provide a framework to guide future investment in the area which it envisages will be through the delivery of a mix of commercial and residential uses. The application site forms the western part of the area identified within the SRF.

An addendum to the SRF was adopted in 2019 in order to reflect the desire of the applicant to develop a new city centre campus at the site. The addendum report states:

“This addendum addresses the opportunity that has arisen for the Manchester College to build its new city centre campus on the site. It promotes educational use on the western part of the site, principally to form a new city centre campus for The Manchester College – the largest provider of further education and vocational training in Greater Manchester as well as significant provider of education. The campus forms a critical part of the college’s estate strategy and will make an essential contribution to the delivery of the skills strategy for the city and city region. The college took legal ownership of the majority of the western part of the site in February 2019”

Great Ducie Street Strategic Regeneration Framework (SRF) (2018)

The Great Ducie Street SRF wraps around the former Boddingtons Brewery SRF. The focus of this SRF is on the surrounding area but under pins the same principles regarding the regeneration of the area into a new, missed use neighbourhood. The SRF advocates the opportunity to facilitate greater synergies between existing businesses in the framework area and emerging development. The vision is to develop a strong sense of place and community, which reflects the principles of the Boddingtons SRF, to deliver residential accommodation balanced by non-residential uses. The vision also sets out that development should significant increase the density within this area to something that is commensurate to the scale of development within the city centre.

City Centre Strategic Plan 2015-2018 (March 2016)

On the 2 March 2016 the City Council's Executive approved the City Centre Strategic Plan which seeks to provide an up-to-date vision for the City Centre within the current economic and strategic context along with outlining the key priorities for the next few years for each City Centre neighbourhood. This document seeks to align itself with the Manchester Strategy (January 2016) along with the Greater Manchester Strategy. Overall the City Centre plan seeks to *“shape the activity that will ensure that the City Centre continues to consolidate its role as a major economic and cultural asset for Greater Manchester and the north of England”*.

It should also be noted that the strategic plan approved by the Executive also endorsed an extended boundary of the City Centre upon which the strategic plan is based. This extended boundary includes the application site.

Manchester Strategy (January 2016)

The strategy sets the long term vision for Manchester's future and how this will be achieved. An important aspect of this strategy is the City Centre and how it will be a key driver of economic growth and a major employment centre. Furthermore, increasing the centre for residential is fundamental along with creating a major visitor destination.

National Planning Policy Framework (2019)

The revised NPPF was adopted in July 2018 and re-issued in February 2019. The document states that the *'purpose of the planning system is to contribute to the achievement of sustainable development'*. The document clarifies that the *'objective of sustainable development can be summarised as meeting the needs of the present without compromising the ability of future generations to meet their own needs'* (paragraph 7).

In order to achieve sustainable development, the NPPF states that the planning system has three overarching objectives – economic, social and environmental (paragraph 8).

Section 6 *'Building a strong and competitive economy'* states that planning decisions should help create the conditions in which businesses can invest, expand and adapt. Significant weight should be placed on the need to support economic growth and productivity, taking into account both local business needs and wider opportunities for development (paragraph 80).

Section 8 *'Promoting Healthy and Safe Communities'* states that *planning policies and decisions should aim to achieve healthy, inclusive and safe places* (paragraph 91).

Section 9 *'Promoting Sustainable Transport'* states that *'significant development should be focused on locations which are or can be made sustainable, through limiting the need to travel and offering a genuine choice of transport modes. This can help to reduce congestion and emissions, and improve air quality and public health'* (paragraph 103).

Developments should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe (paragraph 109).

All developments that will generate significant amounts of movement should be required to provide a travel plan, and the application should be supported by a transport statement or transport assessment so that the likely impacts of the proposal can be assessed (paragraph 111).

Section 11 *'Making effective use of land'* states that *'planning decisions should promote an effective use of land in meeting the need for homes and other uses, while safeguarding and improving the environment and ensuring safe and healthy living conditions'* (paragraph 117). Decisions should support development that

makes efficient use of land, taking into account: the identified need for different types of housing and other forms of development, and the availability of land suitable for accommodating it; local market conditions and viability; the availability and capacity of infrastructure and services – both existing and proposed – as well as their potential for further improvement and the scope to promote sustainable travel modes that limit future car use; the desirability of maintaining an area's prevailing character and setting (including residential gardens), or of promoting regeneration and change; and the importance of securing well-designed, attractive and healthy places. (Paragraph 122).

Where there is an existing or anticipated shortage of land for meeting identified housing needs, it is especially important that planning decisions avoid homes being built at low densities, and ensure that developments make optimal use of the potential of each site. Paragraph 123 (c) states that Local Planning Authorities should refuse applications which they consider fail to make efficient use of land, taking into account the policies in the NPPF. In this context, when considering applications for housing, authorities should take a flexible approach in applying policies or guidance relating to daylight and sunlight, where they would otherwise inhibit making efficient use of a site (as long as the resulting scheme would provide acceptable living standards).

Section 12 '*Achieving Well Designed Places*' states that '*the creation of high quality buildings and places is fundamental to what the planning and development process should achieve. Good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities. Being clear about design expectations, and how these will be tested, is essential for achieving this*' (paragraph 124).

Planning decisions should ensure that developments: will function well and add to the overall quality of the area, not just for the short term but over the lifetime of the development; are visually attractive as a result of good architecture, layout and appropriate and effective landscaping.

In determining applications, great weight should be given to outstanding or innovative designs which promote high levels of sustainability, or help raise the standard of design more generally in an area, so long as they fit in with the overall form and layout of their surroundings (paragraph 131).

Section 14 '*Meeting the challenge of climate change, flooding and coastal change*' states that the planning system should support the transition to a low carbon future in a changing climate, taking full account of flood risk and coastal change. It should help to: shape places in ways that contribute to radical reductions in greenhouse gas emissions, minimise vulnerability and improve resilience; encourage the reuse of existing resources, including the conversion of existing buildings; and support renewable and low carbon energy and associated infrastructure (paragraph 148).

Section 15 '*Conserving and Enhancing the natural environment*' states that planning decision should contribute and enhance the natural and local environment by protecting valued landscapes, minimising impacts on and providing net gains for

biodiversity, preventing new and existing development from contributing to unacceptable levels of soil, air, water or noise pollution or land instability and remediating contaminated land.

Section 16 '*Conserving and enhancing the historic environment*' states that in determining applications, Local Planning authorities should require an applicant to describe the significance of any heritage assets affected, including any contribution made by their setting. The level of detail should be proportionate to the assets' importance and no more than is sufficient to understand the potential impact of the proposal on their significance. As a minimum the relevant historic environment record should have been consulted and the heritage assets assessed using appropriate expertise where necessary. Where a site on which development is proposed includes, or has the potential to include, heritage assets with archaeological interest, local planning authorities should require developers to submit an appropriate desk-based assessment and, where necessary, a field evaluation (paragraph 189).

In determining applications, local planning authorities should take account of:

- a) the desirability of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation;
- b) the positive contribution that conservation of heritage assets can make to sustainable communities including their economic vitality; and
- c) the desirability of new development making a positive contribution to local character and distinctiveness. (Paragraph 192)

In considering the impacts of proposals, paragraph 193 states that the impact of a proposal on the significance of a designated heritage asset, great weight should be given to the asset's conservation (and the more important the asset, the greater the weight should be). This is irrespective of whether any potential harm amounts to substantial harm, total loss or less than substantial harm to its significance.

Paragraph 194 goes on to state that any harm to, or loss of, the significance of a designated heritage asset (from its alteration or destruction, or from development within its setting), should require clear and convincing justification.

Paragraph 196 states that where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal including, where appropriate, securing its optimum viable use.

The effect of an application on the significance of a non-designated heritage asset should be taken into account in determining the application. In weighing applications that directly or indirectly affect non-designated heritage assets, a balanced judgement will be required having regard to the scale of any harm or loss and the significance of the heritage asset (paragraph 197).

Paragraphs 10, 11, 12, 13 and 14 of the NPPF outline a “presumption in favour of sustainable development”. This means approving development, without delay, where it accords with the development plan and where the development is absent or relevant policies are out-of-date, to grant planning permission unless any adverse impacts of doing so would significantly and demonstrably outweigh the benefits when assessed against the NPPF.

Planning Policy Guidance (PPG)

The relevant sections of the PPG are as follows:

Air Quality provides guidance on how this should be considered for new developments. Paragraph 8 states that mitigation options where necessary will be locationally specific, will depend on the proposed development and should be proportionate to the likely impact. It is important therefore that local planning authorities work with applicants to consider appropriate mitigation so as to ensure the new development is appropriate for its location and unacceptable risks are prevented. Planning conditions and obligations can be used to secure mitigation where the relevant tests are met.

Examples of mitigation include:

- the design and layout of development to increase separation distances from sources of air pollution;
- using green infrastructure, in particular trees, to absorb dust and other pollutants;
- means of ventilation;
- promoting infrastructure to promote modes of transport with low impact on air quality;
- controlling dust and emissions from construction, operation and demolition; and
- contributing funding to measures, including those identified in air quality action plans and low emission strategies, designed to offset the impact on air quality arising from new development.

Noise states that Local planning authorities should take account of the acoustic environment and in doing so consider:

- whether or not a significant adverse effect is occurring or likely to occur;
- whether or not an adverse effect is occurring or likely to occur; and
- whether or not a good standard of amenity can be achieved.

Mitigating the noise impacts of a development will depend on the type of development being considered and the character of the proposed location. In general, for noise making developments, there are four broad types of mitigation:

- engineering: reducing the noise generated at source and/or containing the noise generated;
- layout: where possible, optimising the distance between the source and noise-sensitive receptors and/or incorporating good design to minimise noise

transmission through the use of screening by natural or purpose built barriers, or other buildings;

- using planning conditions/obligations to restrict activities allowed on the site at certain times and/or specifying permissible noise levels differentiating as appropriate between different times of day, such as evenings and late at night, and;
- mitigating the impact on areas likely to be affected by noise including through noise insulation when the impact is on a building.

Design states that where appropriate the following should be considered:

- layout – the way in which buildings and spaces relate to each other
- form – the shape of buildings
- scale – the size of buildings
- detailing – the important smaller elements of building and spaces
- materials – what a building is made from

Health and wellbeing states opportunities for healthy lifestyles have been considered (e.g. planning for an environment that supports people of all ages in making healthy choices, helps to promote active travel and physical activity, and promotes access to healthier food, high quality open spaces and opportunities for play, sport and recreation);

Travel Plans, Transport Assessments in decision taking states that applications can positively contribute to:

- encouraging sustainable travel;
- lessening traffic generation and its detrimental impacts;
- reducing carbon emissions and climate impacts;
- creating accessible, connected, inclusive communities;
- improving health outcomes and quality of life;
- improving road safety; and
- reducing the need for new development to increase existing road capacity or provide new roads.

Other legislative requirements

Section 16 (2) of the Planning (Listed Building and Conservation Areas) Act 1990 (the "Listed Building Act") provides that "in considering whether to grant listed building consent for any works to a listed building, the local planning authority or the Secretary of State shall have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses"

Section 66 Listed Building Act requires the local planning authority to have special regard to the desirability of preserving the setting of listed buildings. This requires more than a simple balancing exercise and considerable importance and weight should be given to the desirability of preserving the setting. Members should consider whether there is justification for overriding the presumption in favour of preservation.

Section 72 of the Listed Building Act provides that in the exercise of the power to determine planning applications for land or buildings within a conservation area, special attention shall be paid to the desirability of preserving or enhancing the character or appearance of that area.

Environmental Impact Assessment

The applicant has submitted an Environmental Statement in accordance with the Town and Country Planning (Environmental Impact Assessment) (England and Wales) Regulations 2017 and has considered the following topic areas:

- Socio-economic impacts;
- Townscape and visual impact;
- Transport and access;
- Air quality and dust;
- Cumulative impacts.

The Proposed Development is an “Infrastructure Project” (Schedule 2, 10 (b)) as described in the EIA Regulations. The Site covers an area of approximately 0.88 hectares and although below the threshold of 1 hectares of development which is not a dwellinghouse, an EIA has been undertaken covering the topic areas above as there are judged to be significant environmental impacts as a result of the development and the change from a car park.

The EIA has been carried out on the basis that the proposal could give rise to significant environmental effects.

In accordance with the EIA Regulations, this ES sets out the following information:

- A description of the proposal comprising information about its nature, size and scale;
- The data necessary to identify and assess the main effects that the proposal is likely to have on the environment;
- A description of the likely significant effects, direct and indirect on the environment, explained by reference to the proposals possible impact on human beings, water, air, climate, cultural heritage, townscape and the interaction between any of the foregoing material assets;
- Where significant adverse effects are identified with respect to any of the foregoing, mitigation measures have been proposed in order to avoid, reduce or remedy those effects; and
- Summary, in non-technical language, of the information specified above.

It is considered that the environmental statement has provided the Local Planning Authority with sufficient information to understand the likely environmental effects of the proposals and any required mitigation.

Issues

Principle of the redevelopment of the site and contribution to regeneration

Regeneration is an important planning consideration. The Boddingtons Strategic Regeneration Framework includes a city centre college campus and residential and commercial development. This is consistent with policies SP1 and EC1 of the Core Strategy which seeks to ensure that high quality and accessible educational facilities are provided in the city centre which is essential to developing the skills for the future workforce and therefore the economic growth of the city and wider region.

Planning permission has been granted for a residential and commercial development and public realm and this college would develop a key plot in the SRF area.

Manchester City Centre is the primary economic driver in the City Region. It is essential for the City Centre to continue to support educational facilities in highly sustainable locations. This emphasis on supporting education uses is outlined in section 8 of the NPPF which requires Local planning authorities should take a proactive, positive and collaborative approach to meeting educational requirements.

The facility would significantly increase the scale and quality of learning provision in the region, with an envisaged 4,000 students enrolled at the campus. This would help support economic growth in the city and regional centre and help deliver the skills required for a future workforce which would support economic and employment growth.

The applicant's estate strategy will consolidate 14 locations into five main campuses. This will release the surplus sites for new housing for around 700 new homes.

The site is directly opposite Victoria train and tram station which makes it highly sustainable and suitable for educational and would be within walking distance of amenities in the heart of the city centre.

The proposal would create 147 full time equivalent construction jobs during the build period and a further 172 off site indirect jobs throughout the supply chain. The GVA associated with these jobs will be £14.3 million per year. There would be opportunities for local people to access employment during the construction period which would be secured by a local labour agreement.

Once in operation, it is estimated that a total of 1,343 full time equivalent jobs would have been created. This would support 270 full time equivalent jobs within the supply chain. The GVA associated within these jobs is estimated to be around £50.8 million per year. There are also direct benefits for the Cheetham ward with 363 full time equivalent jobs estimated to be generated locally which has a GVA of £11.1 million.

The college and spending from students would have a positive impact on local businesses in terms of retail, food service and transport which will bring additional spending onto the local area. The additional spending would bring jobs and revenue to the local area.

The socio economic benefits of the scheme have been considered as part of the Environmental Statement. This concludes that the impact of the development would be positive and the proposal would allow the future work force to gain the skills and education required to support economic growth in the city.

The investment made in delivering this next phase of the former Boddingtons SRF would help to unlock the wider SRF and continue the success, economic growth and job creation in this part of the city centre.

The proposal would be of the highest quality design, be energy efficient and provide further investment in the public realm which would create pedestrian routes and improve connectivity to the city centre and future development around the area.

The development would therefore be consistent with the City Centre Strategic Plan and would complement and build upon the City Council's current and planned regeneration initiatives and as such would be consistent with sections 1 and 2 of the National Planning Policy Framework, and Core Strategy policies SP1, EC1, CC1, CC6, CC7, CC8, CC9, CC10, EN1 and DM1. As such, it is necessary to consider the potential impact of the development.

Impact of the historic environment and cultural heritage

The site is not within a Conservation Area but there are a number of Listed Buildings in close proximity that could be affected by the development.

The urban grain around the site is a mixture of low quality surface level car parks and cleared sites, plus large scale buildings such as the Manchester Arena and other residential and commercial city centre buildings.

The most notable nearby listed buildings are the Tower of HMP Manchester, main prison block and gatehouse, all of which are grade II, approximately 320 metres from the site and the Manchester Parcel Post Office (grade II) which is approximately 100 metres. Victoria Train station (grade II) is in close proximity but is separated and screened from the site by Manchester Arena.

There are other structures close to the site including: Cathedral Church of St Mary (Grade I), Cheetham Hospital (Grade I), Roman Catholic Church of St Chad (grade II), Former Synagogue (grade II), Former corn exchange (grade II), detached block of school room (grade II), Fragment of Hydes Cross (grade II), South east wing of Cheethams Hospital (grade II), North Bridge (grade II), Middle Bridge (grade II) and Stephenson Bridge (grade II). However, the distances and urban grain ensure that development would not have a material impact on these assets. The site is close to the Cathedral and Shudehill Conservation Areas which are 170 metres and 320 metres south east of the site respectively.

Given the overall scale of the proposal at 5 storeys, and the distances between the listed buildings and conservation areas and the site, the townscape and visual impact assessment has confirmed that there would be no material impact on the setting of these designated assets.

The proposal would improve the local environment by removing a surface car park site along a major road route in the city centre. The proposal provides an opportunity to enhance the street scene with a building of architectural quality along the inner ring road and Great Ducie Street. This would bring activity and vitality to the local area and improve the local context including the setting of nearby heritage assets.

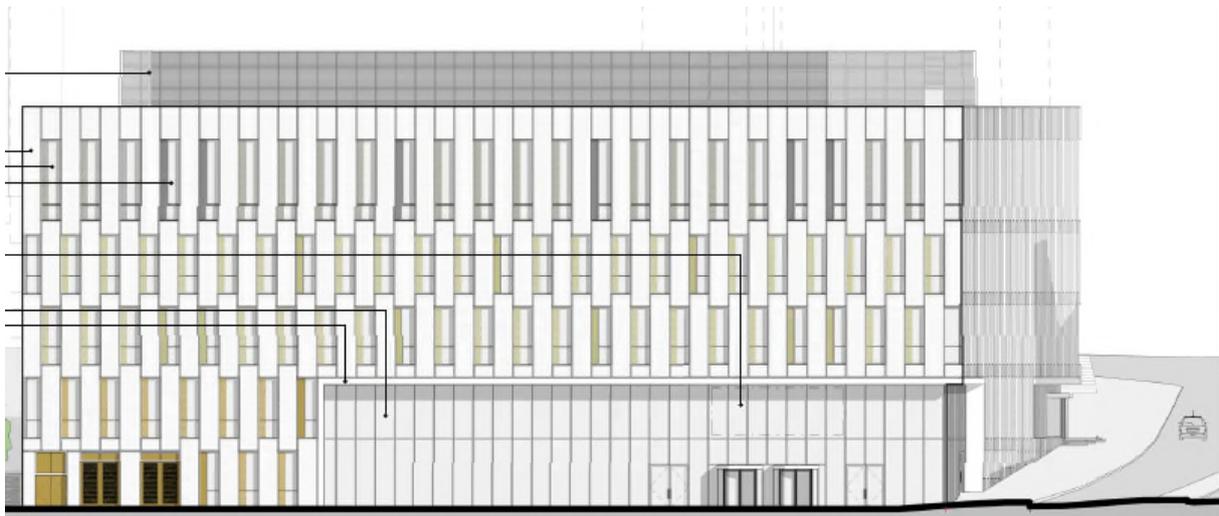
Impact on Archaeology

An archaeological assessment notes that archaeology may exist and that the existing building on site have some local significance. GMAAS consider that it would be appropriate to undertake a programme of works on any remains and ground excavations to record the archaeology. Following completion of the works, and depending on the quality of the archaeological investigations, there should be some form of commemoration of the remains. A condition should be imposed to this affect to satisfy the requirements of policy EN3 of the Core Strategy and saved policy DC20 of the UDP.

Visual amenity and impact on townscape

The development would be a rectangular block creating active frontages to New Bridge Street and Great Ducie Street. The main entrance for UCEN would be from New Bridge Street and Manchester College from Great Ducie Street. The layout would enhance the inner ring road and surrounding road network and activate the boundary with new public realm provided at the Old Brewery Gardens.

The building would be 5 storeys with a uniform massing across the building.



Great Ducie Street elevation



New Bridge Street elevation

The scale and massing would be appropriate within an urban grain that is fragmented with buildings of varying scale and quality. The scale of the building, holding the corner of New Bridge Street and Great Ducie Street would enhance the street scene. Its appearance would be of the highest quality and would support the ongoing regeneration and revitalisation of this part of the city centre.

The main material would be a dark brick with a crisp, engineered appearance. This would contrast with the red brick façade of Old Brewery Gardens, distinguishing the educational building from the traditional red brick of the residential development.

The façade has a modular pattern with alternating windows on each floor. Recessed joints between the panel like elements would create a weaved appearance. Columns would be concealed behind these panels. For internal spaces that do not require daylight, the glazing would be replaced with a spandrel panel to maintain the simplicity of the regular façade.

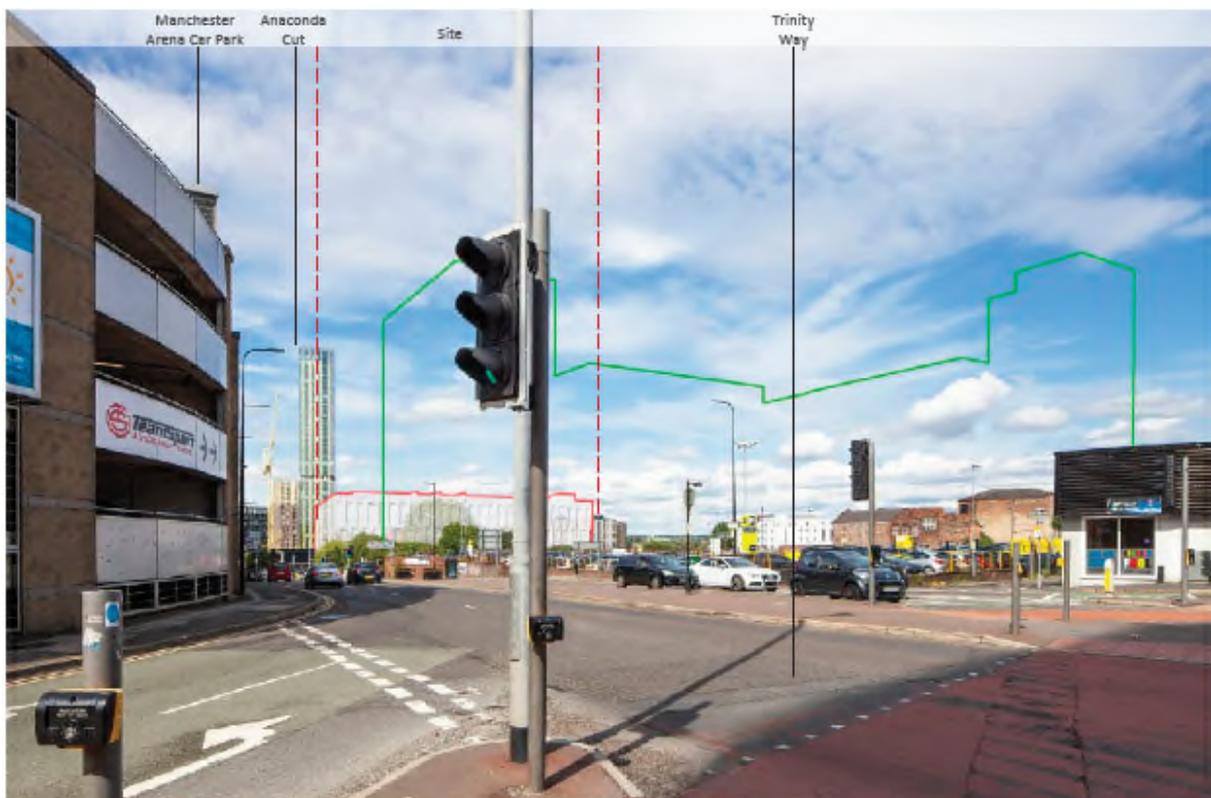
Six key views of the site have been considered as part of the Environmental Statement. These are as follows:

- View one – junction of Cheetham Hill Road and New Bridge Street looking westwards;
- View two – junction of Great Ducie Street and Southall Street looking south-eastwards;
- View three – from the pedestrian access to Cathedral Gardens off Victoria Street (adjacent to Manchester Cathedral) looking northwards;
- View four – junction of Victoria Street and Hunts Bank (adjacent to Cheetham’s Library) looking northwards;
- View five – junction of Trinity Way and Spinningfield Lane looking eastwards;
- View Six – from public footpath adjacent to the Irwell Riverside Housing development looking eastwards.

View One

The view is relatively open with mid to long range views of the urban area. The view is dominated by highway infrastructure, Manchester Arena and the surface car park. The only building of notable scale in the view is the Anaconda Cut tower block at the edge of Greengate. Great Ducie Street is one of the main approaches to the city centre, however, the view is generally dominated by a low value townscape.

The proposal would clearly be visible but given its scale, the other taller buildings would remain the dominant features. The proposal would lead to an enhancement of the view through the removal of the surface car park and the provision of a high quality building.



View one

View two

The view is characterised by a variety of buildings of differing scale and architecture and highway infrastructure and is characterised by low value features and townscape.

The proposal would be glimpsed behind the Travelodge and there would be no perceptible change to the character and quality of this view.

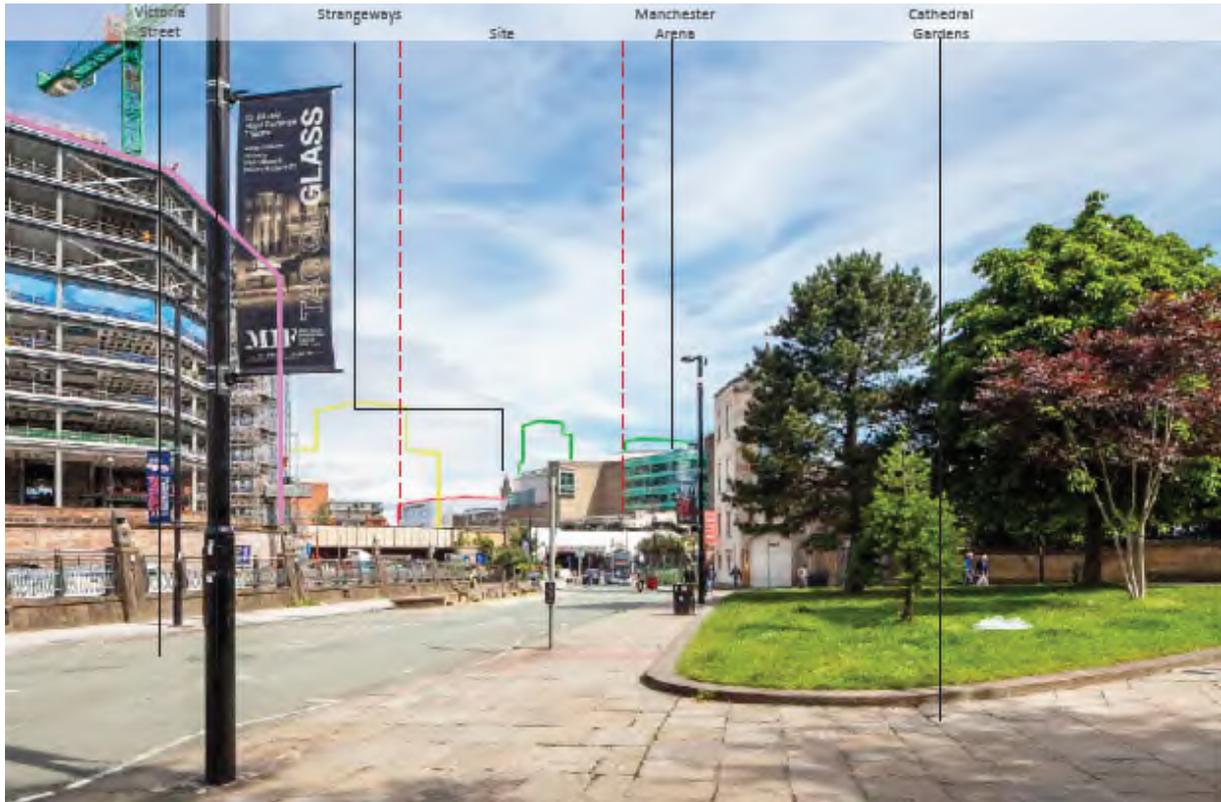


View two

View three

This view is from the footway adjacent to Manchester Cathedral (Grade I) and is sensitive given the setting of the Cathedral and Cathedral Gardens. The railway viaduct is also largely in view together with the Manchester Arena. There is also a glimpsed view of the Grade II tower and main block at the prison behind the arena.

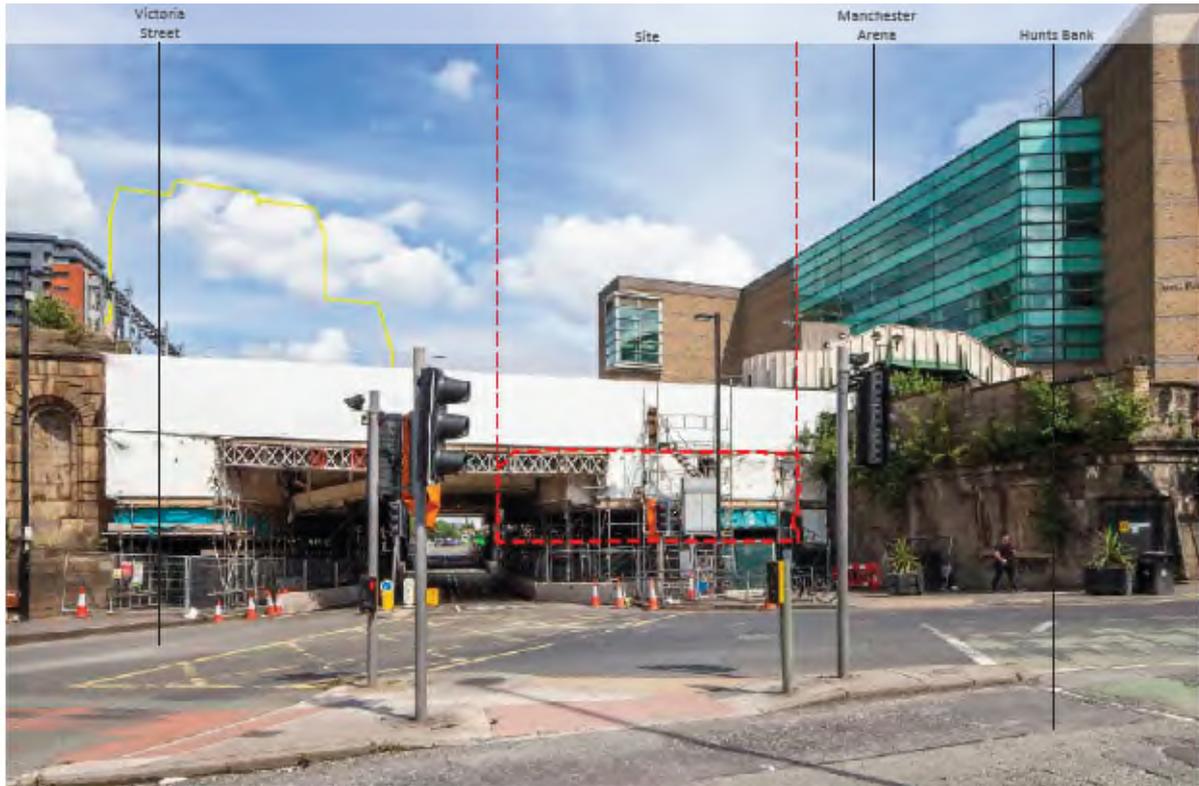
The proposal would be partially visible beyond Manchester Arena and would extend above the railway viaduct. Although visibility of the building will be limited, it would partially obscure the listed tower to the prison. However, given the limited views of the listed assets, the partial loss is not considered to be unduly harmful on the character and quality of the view and the heritage assets within it.



View three

View four

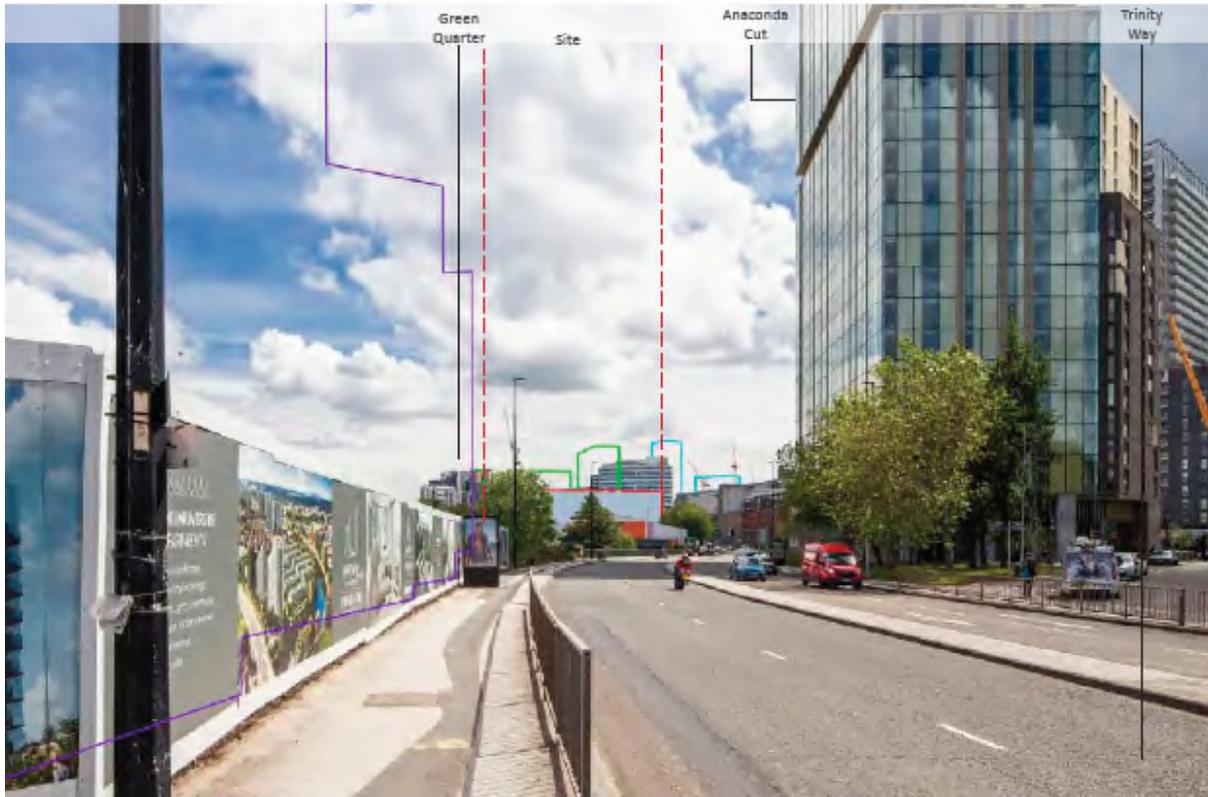
The view is taken from the footpath at the junction of Victoria Street and Hunts Bank with the Grade I Listed Chetham library which is elevated above the footpath. It is dominated by the railway viaduct, Manchester Arena and highway infrastructure. The railway viaduct obscures all view of the proposed building and therefore there would be no perceptible change to this view.



View four

View five

The view on the footpath at the junction of Trinity Way and Spiningfields Lane, is dominated by highway infrastructure and the 44 storey Anaconda tower. The left of the view is the hoarding line for the Uptown development together with views along Trinity Way. Several tall apartment and office buildings form the backdrop. Whilst the proposal would be visible, it would be a relatively modest addition to the street scene. The character and appearance of the view would be enhanced through the addition of a building of the architectural quality proposed.



View five

View six

The view is on the western bank of the River Irwell adjacent to the Irwell Riverside residential development. It comprises vegetation with the 20 storey Tempus tower forming a prominent feature with glimpsed view of the Peninsula building. The proposal would project above a number of 2-3 storey industrial buildings along Great Ducie Street, but the rooflines at the Greenquarter would remain dominant. Tree coverage and vegetation would limit the visibility of the building and the character of visual amenity of this view would not alter



View six

The proposal would be relatively modest within a context of much larger buildings. The assessment demonstrates that the building would largely be positive and beneficial to the townscape and would not harm heritage assets or sensitive views.

The impact of the height would not be unduly harmful. Indeed and in the majority of instances the impacts on the local area and on the city townscape would be positive. The use of a high quality materials, and the creation of a distinctive development, would result in an acceptable scheme.

Where the development would clearly be seen in the same context as heritage assets, the significance and setting of them is clearly still evident and any harm that does arise would be modest and outweighed by the substantial regeneration benefits that the development of such a high quality scheme would deliver.

Ecology

An ecological appraisal concludes that the development would not result in any significant or unduly harmful impacts to local ecology given the current condition of the site as car park. Greater Manchester Ecology Unit concur with the findings.

The proposals would require the removal of a laurel and hawthorn shrubs at the front of the site which is in a poor condition. The loss of this modest amount of green infrastructure is suitably mitigated against through the creation of the landscaped roof garden and street trees along New Bridge Street, together with other

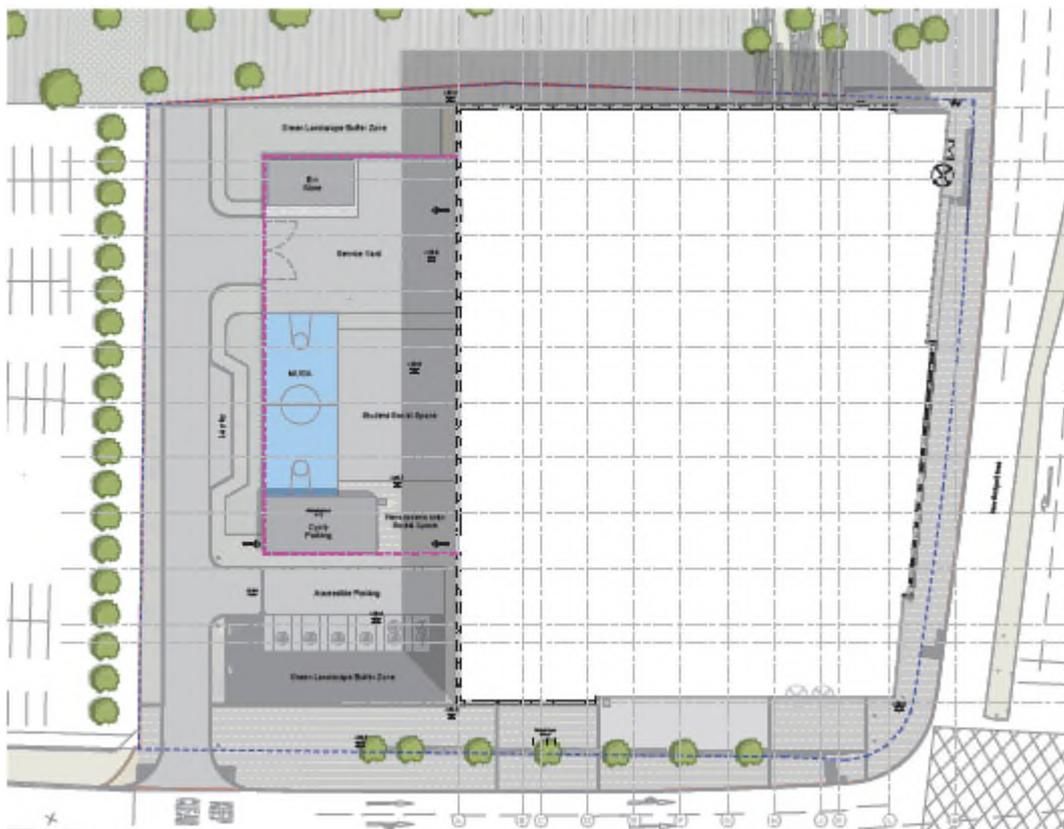
landscaping, would enhance green infrastructure and biodiversity at the site and in the area in line with policy EN9 of the Core Strategy.

The roof gardens in particular are large enough to include trees, shrubs and other vegetation which will encourage habitats for wildlife therefore improving the overall biodiversity of the development

Contribution to Improving Permeability, Public Spaces and Facilities and Provision of a Well Designed Environment

The public realm around the site would be improved. The building line along Great Ducie Streets creates a generous pavement width and allows 7 street trees to be planted. This would enhance the setting of the building and define the entrance to the college.

The access road and parking area for disabled people at the northern end of the site would be softened by a landscaped buffer. This would promote permeability and biodiversity and enhance the setting of the college and Old Brewery Gardens.



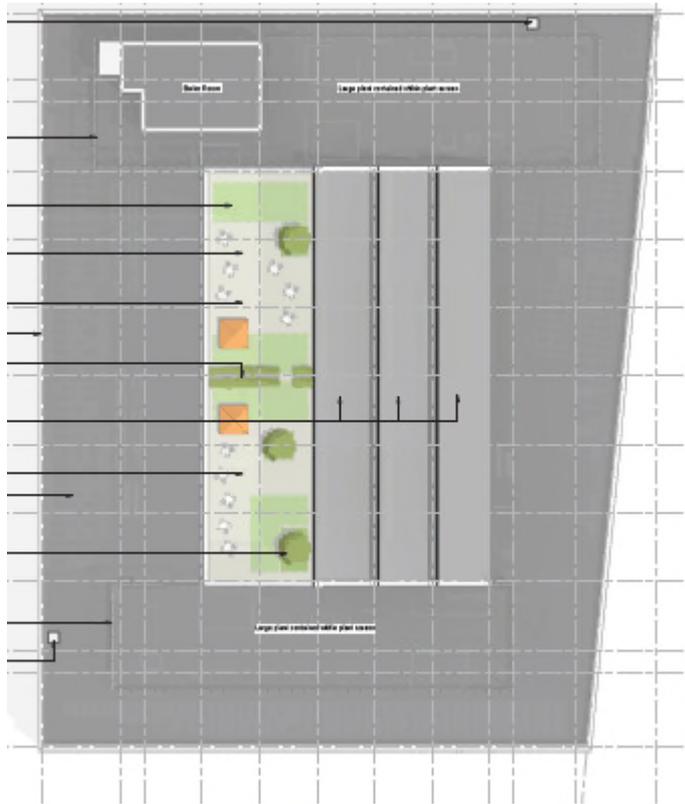
Landscape and recreational areas

A student social space, multi-use games area, cycle store, refuse store and servicing area would be at the rear of the building. These facilities provide functional back of house facilities as well as social space for students, staff and visitors.

Once the second phase of development takes place, which will expand the college, these facilities, in the current proposed location, will be removed to make way for the

next phased of development. Enhanced facilities will be provided within this next phase to cater for any additional demands that the expanded floor space will have on these functional and social facilities.

The social facilities would also include separate roof top terraces for both colleges.



Roof terraces

Effect of the development on the local environment and existing residents

(a) Sunlight, daylight, overshadowing and overlooking

It is considered that the impacts on the daylight and sunlight availability on surrounding properties is not considered to be unduly harmful. The majority of the more significant impacts are on the surrounding low rise commercial buildings, particularly those located to the north of the site and the Travelodge. These properties are not considered to be highly sensitive due to their commercial nature and are adequately separated from the site by the road network.

The Old Brewery Gardens residential development would be to the east of the site. A sun path analysis demonstrates that there would be no significant impact because residential accommodation is higher than the college. The distance between the east elevation of the college and the west elevation of the Old Brewery Gardens development is approx. 33 metres and would ensure no unduly harmful impacts.

(b) TV reception

A TV reception survey has concluded that there is likely to be minimal impact on digital television services or digital satellite television services but should any arise it could be mitigated through antenna upgrade or realignment of the transmitter. A condition would require of a post completion survey to be undertaken to verify that this is the case and that no additional mitigation is required.

(a) Air quality

The site is within an Air Quality Management Area (AQMA) where air quality conditions are known to be poor as a result of vehicular emissions. An air quality report notes that during the construction phases there could be impact from dust, earth works/construction and vehicle emissions which would be minimised through good practice which should remain in place for the duration of the works and should be a condition.

There would be no onsite parking for staff or students who should take advantage of the highly sustainable location. 112 space secure cycle store would be created. 6 parking spaces, plus two flexible would be provided for disabled people and would be fitted with an electric car charging point.

Environmental Health concur with the conclusions and recommendations within the air quality report. The mitigation measures would be secured by planning condition and the proposal would comply with policy EN16 of the Core Strategy, paragraph 8 of the PPG and paragraph 124 of the NPPF in that there will be no detrimental impact on existing air quality conditions as a result of the development.

Fume extraction

Fume extraction would be required for the commercial units if they are to be occupied by a food and drink use. It is considered that a suitable scheme can be put in place and integrated into the scheme. In this regard, it is recommended that a condition of the planning approval is that the fume extraction details are agreed.

Noise and disturbance

A noise assessment identifies the main sources of noise would be from: plant and construction activities. The acoustic specification would limit noise ingress from external noise, particularly from nearby roads.

Noise levels from the construction would be acceptable provided that the strict operating and delivery hours are adhered to along with the provision of an acoustic site hoarding, equipment silencers and regular communication with nearby residents. This should be secured by a planning condition.

The main source of noise to the educational building will be from traffic on the surrounding roads. Appropriate glazing would ensure that noise levels within the building are acceptable and that there is no harmful noise outbreak from the building particularly from the social spaces and theatre.

Provided that construction activities are carefully controlled and the plant equipment and accommodation are appropriately insulated the proposal would be in accordance with policy DM1 of the Core Strategy, extant policy DC26 of the UDP and the NPPF.

Waste management

The waste would be segregated in a dedicated waste storage area at the rear of the building. The college is committed to recycling and appropriate waste management. The refuse store could accommodate 22,340 litres of waste with approximately 78,820 litres taken off site each week.

The following waste requirements will be required at the college:

- General waste – 1 x 1100 litre units (55 units per week collected);
- Bulky waste – 3.4 m x 1.8 m skip collected on request;
- Paper/card recycling – 5 x 1100 litre units (10 units per week collected);
- Co-mingled – 3 x 1100 litre units (6 units per week collected);
- Food recycling – 3 x 240 litre units (1.5 units per week collected); and
- Glass recycling – 3 x 240 litre units (1.5 units per week collected).

The waste stores would meet Council standards and will form part of the conditions of the planning approval.

Accessibility

The proposal would be accessible with six parking spaces for disabled people plus a further two spaces within the layby area (8 in total) and level access into the building with lift access to all floors.

Impact on the highway network/transport issues

There is a range of transport modes nearby and the site is close to amenities and services. Manchester Victoria train and Metrolink station is situated directly adjacent to the application site.

The transport assessment indicates that the proposal would have a minimal impact on the surrounding highway network.

There will be no on site car parking for the college expect for 6 disabled car parking spaces plus two further flexible spaces within the layby off the new access road creating 8 disabled spaces in total to serve the development.

This approach to car parking is acceptable given the highly sustainable location of the application site. The needs of disabled staff and students have been taken into account through the provision of an appropriate level of disabled car parking.

The majority of the 112 cycle parking spaces would be within dedicated secure cycle stores. There will also be provision within the public realm in the form of hoops. The

level of cycle provision at the college will be reviewed once the college becomes operational and if there is demand for further spaces these will be provided.

A travel plan would be prepared and its full implementation should form part of the conditions of any planning approval.

Servicing will take place via a new dedicated access road to a servicing yard at the rear of the building. An operational management plan will be required to ensure that the servicing arrangements are acceptable.

The location of the new entrance to the college off Trinity Way will require pedestrian improvements at the junction of Great Ducie Street and Trinity Way. This will ensure that there is a safe pedestrian environment for students and staff given the uplift in movements as a result of the new entrance.

These measures would all be secured by planning condition.

The proposal therefore accords with policies SP1, T1, T2 and DM1 of the Core Strategy.

Flood Risk/surface drainage

The majority of the site is in Flood Zone 2 'medium probability flood risk' and is within a critical drainage area where there are complex surface water flooding problems from ordinary watercourses, culverts and flooding from the sewer network. These areas are sensitive to an increase in the rate of surface water run off and/or volume from new developments which may exasperate local flooding problems.

Due to the sites location in flood zone 2, the proposal has to satisfy the requirements of the Sequential Test and where applicable the Exception test as outlined in the NPPF and NPPG.

The educational use of the site classifies the development as 'more vulnerable'. However, there are no records of flooding at the site and an appraisal of the flood risk potential confirms the development is appropriate in terms of the sequential test. As a consequence, the exception test is not required.

The application has been assessed by Flood Risk Management Team who require a detailed drainage scheme through a condition along with a management/verification plan. In addition, the finished floor levels should be set at or above 28.760m AOD and ground levels over the rest of the site must not be raised to ensure that there is no risk of flooding to other sites.

In order to satisfy the provisions of policy EN14 of the Core Strategy, it is recommended that the drainage plan and floor levels forms part of the conditions of the planning approval.

Designing out crime

A Crime Impact Statement (CIS), prepared by Design for Security at Greater Manchester Police, recognises that the development would bring vitality to this area and more active frontage. It is recommended that a condition of the planning approval is that the CIS is implemented in full as part of the development in order to achieve Secured by Design Accreditation.

Ground conditions

A ground conditions report provides notes that assessment is required to inform the final remediation strategy. A verification report should confirm that the agreed remediation has been carried out. This approach should form a condition of the planning approval in order to comply with policy EN18 of the Core Strategy.

Climate change, sustainability and energy efficiency

The proposal would be a low carbon building in a highly sustainable location with excellent access to public transport for students, staff and visitors.

Sustainability principles would be incorporated into the construction process to minimise and recycle waste, efficiency in terms of vehicle movements and sourcing and use of materials.

There would be no on site car parking, apart from six bays for disabled people and two flexible disabled bays within the layby. These bays would be fitted with an electric charging point. Vehicle emissions at the site would be limited and should not contribute to local air quality conditions.

A travel plan would encourage students and staff to use public transport and minimise vehicular trips. A secure cycle store would provide 112 spaces for staff and students. There is a commitment to review this within 12 months of occupation to determine if there is greater demand for cycling than anticipated.

The building fabric would be highly efficient with energy saving measures incorporated into the design. Approximately 500 sqm of photovoltaic panels would be installed on the roof. Other measures would be included such as LED lighting and highly efficient water heaters, boilers and air source heat pumps.

As per the requirements of policy EN6 of the Core Strategy, developments must achieve a minimum 15% reduction in CO2 emissions (i.e. a 15% increase on Part L 2010). Since the Core Strategy was adopted, Part L 2010 has been superseded by Part L 2013 which has more stringent energy requirements. The 15% requirements translates as a 6% improvement over Part L 2013. The Environment standards statement for this building states that the CO2 emissions from the building will be reduced by 35.16% thereby meeting the requirements of policy EN6. The proposal is expected to achieve BREEAM excellent in line with policy DM1 of the Core Strategy.

Green infrastructure would include landscaping to the roof terraces and green buffers together with new street tree planting on New Bridge Street.

Conclusion

The proposal would develop a brownfield site within an important regeneration area for a city centre educational campus for Manchester College. The building would support economic growth by providing students with the skills required to be an active member of the future workforce.

Careful consideration has been given to the siting, scale and appearance of the development to ensure it provide a high quality development. The development will be fully accessible and will provide onsite disabled parking for those that require it.

The proposal will contribute towards place making in the area with new public realm, street trees and new landscaping in order to contribute to the setting of the inner ring road. The proposal will also contribute positively to climate change with a highly sustainable building fabric, solar panels and other energy efficiency measures.

Human Rights Act 1998 considerations – This application needs to be considered against the provisions of the Human Rights Act 1998. Under Article 6, the applicants (and those third parties, including local residents, who have made representations) have the right to a fair hearing and to this end the Committee must give full consideration to their comments.

Protocol 1 Article 1, and Article 8 where appropriate, confer(s) a right of respect for a person's home, other land and business assets. In taking account of all material considerations, including Council policy as set out in the Core Strategy and saved policies of the Unitary Development Plan, the Director of Planning, Building Control & Licensing has concluded that some rights conferred by these articles on the applicant(s)/objector(s)/resident(s) and other occupiers and owners of nearby land that might be affected may be interfered with but that that interference is in accordance with the law and justified by being in the public interest and on the basis of the planning merits of the development proposal. She believes that any restriction on these rights posed by the approval of the application is proportionate to the wider benefits of approval and that such a decision falls within the margin of discretion afforded to the Council under the Town and Country Planning Acts.

Recommendation APPROVE

Article 35 Declaration

Officers have worked with the applicant in a positive and proactive manner based on seeking solutions to problems arising in relation to dealing with the planning application. Pre application advice has been sought in respect of this matter where early discussions took place regarding the siting/layout, scale, design and appearance of the development along with noise and highways matters. The proposal is considered to be acceptable and therefore determined within a timely manner.

Reason for recommendation
Conditions to be attached to the decision

1) The development must be begun not later than the expiration of three years beginning with the date of this permission.

Reason - Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990.

2) The development hereby approved shall be carried out in accordance with the following drawings and documents:

Drawings

1092 Rev P01, 1091 Rev P01, 1093 Rev P01, 1090 Rev P01, 1094 Rev P01, 1095 Rev P01, 2051 Rev P02, 2296 Rev P01, 4292 Rev P02, 4291 Rev P01, 3294 Rev P01, 3293 Rev P01, 3292 Rev P01, 3291 Rev P01, 4592 Rev P01, 4591 Rev P01, 3495 Rev P01, 3494 Rev P01, 3493 Rev P01, 3492 Rev P01, 3396 Rev P01, 3395 Rev P01, 3394 Rev P01, 3393 Rev P01, 3392 Rev P01, 3391 Rev P01, 3390 Rev P01, 7749 Rev P01 and 0091 rev P01

All stamped as received by the City Council, as Local Planning Authority, on the 1 October 2019

Supporting information

Design and Access statement, Environmental Statement (Volumes 1, 2 and 3), Archaeological desk based assessment, Crime Impact Statement, Environmental Standards Statement, Manchester College Statement, Planning Statement, TV reception study, ventilation statement, construction management plan, archaeological watching brief, acoustic report, arboricultural impact assessment, ecological appraisal, ground conditions report and waste management strategy.

All stamped as received by the City Council, as Local Planning Authority, on the 1 October 2019

Highways technical note stamped as received by the City Council, as Local Planning Authority, on the 3 December 2019

Revised Acoustic assessment Rev 03, revised construction management plan, revised ecological appraisal Rev A stamped as received by the City Council, as Local Planning Authority on the 28 November 2019

Reason - To ensure that the development is carried out in accordance with the approved plans. Pursuant to policies SP1 and DM1 of the Core Strategy.

3) No development groundworks shall take place until the applicant or their agents or successors in title has secured the implementation of a programme of archaeological works. The works are to be undertaken in accordance with a Written Scheme of

Investigation (WSI) submitted to and approved in writing by Manchester Planning Authority. The WSI shall cover the following:

1. A phased programme and methodology of investigation and recording to include:
 - i) archaeological evaluation through trial trenching
 - ii) dependent on the above, open area excavation and recording (subject to a separate WSI)
2. A programme for post investigation assessment to include:
 - production of a final report on the significance of the below-ground archaeological interest.
3. Deposition of the final report with the Greater Manchester Historic Environment Record.
4. Dissemination of the results of the archaeological investigations commensurate with their significance.
5. Provision for archive deposition of the report and records of the site investigation.
6. Nomination of a competent person or persons/organisation to undertake the works set out within the approved WSI.

Reason: To ensure appropriate investigation of the onsite archaeology pursuant to policy EN3 of the Manchester Core Strategy (2012), saved policy DC20 of the Unitary Development Plan for the City of Manchester (1995) and the NPPF

4) Notwithstanding the revised construction management plan stamped as received by the City Council, as Local Planning Authority, on the 28 November 2019, prior to the commencement, a detailed construction management plan outlining working practices shall be submitted to and approved in writing by the local planning authority, which for the avoidance of doubt should include;

- Display of an emergency contact number;
- Details of Wheel Washing;
- Dust suppression measures;
- Consultation with local residents;
- Compound locations where relevant;
- Location, removal and recycling of waste;
- Routing strategy and swept path analysis;
- Parking of construction vehicles and staff; and
- Sheeting over of construction vehicles.

The development shall be carried out in accordance with the approved construction management plan.

Reason - To safeguard the amenities of nearby residents, highway safety and air quality, pursuant to policies SP1, EN16, EN19 and DM1 of the Manchester Core Strategy (July 2012).

5) a) Prior to the commencement of the development, details of a Local Benefit Proposal, in order to demonstrate commitment to recruit local labour for the duration of the construction of the development, shall be submitted for approval in writing by

the City Council, as Local Planning Authority. The approved document shall be implemented as part of the construction of the development.

In this condition a Local Benefit Proposal means a document which includes:

- i) the measures proposed to recruit local people including apprenticeships
- ii) mechanisms for the implementation and delivery of the Local Benefit Proposal
- iii) measures to monitor and review the effectiveness of the Local Benefit Proposal in achieving the objective of recruiting and supporting local labour objectives

(b) Within one month prior to construction work being completed, a detailed report which takes into account the information and outcomes about local labour recruitment pursuant to items (i) and (ii) above shall be submitted for approval in writing by the City Council as Local Planning Authority.

Reason – The applicant has demonstrated a commitment to recruiting local labour pursuant to policies SP1, EC1 and DM1 of the Manchester Core Strategy (2012).

6) The development shall be carried out in accordance with the flood risk assessment and drainage strategy stamped as received by the City Council, as Local Planning Authority, on the 1 October 2019. For the avoidance of doubt this shall include finished floor levels at the building being set at or above 28.760m AOD and ground levels over the rest of the site must not be raised to ensure that there is no risk of flooding to other sites.

Reason – In the interest of managing the flood risk at the development pursuant to policy EN14 of the Manchester Core Strategy (2012).

7) Notwithstanding the flood risk assessment and drainage strategy stamped as received by the City Council, as Local Planning Authority, on the 1 October 2019, (a) the development shall not commence until a scheme for the drainage of surface water for the development has been submitted for approval in writing by the City Council as the Local Planning Authority. This shall include:

- Details of surface water attenuation that offers a reduction in surface water runoff rate in line with the Manchester Trafford and Salford Strategic Flood Risk Assessment, i.e. at least a 50% reduction in runoff rate compared to the existing rates, as the site is located within Conurbation Core Critical Drainage Area;
- Runoff volume in the 1 in 100 year, 6 hours rainfall shall be constrained to a value as close as is reasonably practicable to the greenfield runoff volume for the same event, but never to exceed the runoff volume from the development site prior to redevelopment
- Evidence that the drainage system has been designed (unless an area is designated to hold and/or convey water as part of the design) so that flooding does not occur during a 1 in 100 year rainfall event with allowance for climate change in any part of a building
- Assessment of overland flow routes for extreme events that is diverted away from buildings (including basements). Overland flow routes need to be designed to convey the flood water in a safe manner in the event of a blockage or exceedance of

the proposed drainage system capacity including inlet structures. The flood water should be routed away from the buildings and towards the less vulnerable areas i.e. open spaces, car parks and roads. A layout with overland flow routes to be presented with appreciation of these overland flow routes with regards to the properties on site and adjacent properties off site.

- Hydraulic calculation of the existing and proposed drainage system;
- Construction details of flow control and SuDS elements.

(b) The development shall then be constructed in accordance with the approved details, within an agreed timescale.

Reason - To promote sustainable development, secure proper drainage and to manage the risk of flooding and pollution pursuant to policies SP1, EN14 and DM1 of the Manchester Core Strategy (2012).

8) Notwithstanding the ground conditions investigation report prepared by Aecom report stamped as received by the City Council, as Local Planning Authority, on the 1 October 2019, (a) before the development hereby approved commences (excluding demolition), the following information shall be submitted for approval in writing by the City Council, as Local Planning Authority:

- Submission of site investigation proposals;
- Submission of a site investigation and risk assessment report;
- Site investigation and risk assessment report;
- Remediation strategy.

Once approved, the development shall then be carried out in accordance with the approved details.

(b) When development commences, the development shall be carried out in accordance with the previously agreed Remediation Strategy and a Completion/Verification Report shall be submitted to and approved in writing by the City Council as local planning authority prior to the first occupation of the development.

In the event that ground contamination, groundwater contamination and/or ground gas, not previously identified, are found to be present on the site at any time before the development is occupied, then development shall cease and/or the development shall not be occupied until, a report outlining what measures, if any, are required to remediate the land (the Revised Remediation Strategy) is submitted to and approved in writing by the City Council as local planning authority and the development shall be carried out in accordance with the Revised Remediation Strategy, which shall take precedence over any Remediation Strategy or earlier Revised Remediation Strategy.

Reason – There is evidence of site contamination at the application site which requires further consideration and examination. In particular, details of outstanding gas monitoring is required to be submitted for consideration and an appropriate remediation strategy prepared. This is pursuant to policy EN18 of the Manchester Core Strategy (2012).

9) Prior to the commencement of development, samples and specifications of all material to be used on all external elevations of the development shall be submitted for approval in writing by the City Council, as Local Planning Authority. The specification shall include the agreement of a materials panel which shall include samples and specifications of all materials to be used on all external elevations of the development along with window reveals, jointing and fixing details, details of the drips to be used to prevent staining, ventilation/louvre details, air bricks and a strategy for quality control management.

The approved materials used shall then be implemented as part of the development.

Reason - To ensure that the appearance of the development is acceptable to the City Council as local planning authority in the interests of the visual amenity of the area within which the site is located, as specified in policies SP1 and DM1 of the Core Strategy.

10) No vegetation clearance shall take place during the optimum period for bird nesting (March - September inclusive) unless nesting birds have been shown to be absent, or, a method statement for the removal works including for the protection of any nesting birds is agreed in writing by the City Council, Local Planning Authority. Any method statement shall then be implemented for the duration of the demolition works.

Reason - In order to protect wildlife from works that may impact on their habitats pursuant to policy EN15 of the Manchester Core Strategy (2012).

11) Prior to the first use of the development, details of the implementation, maintenance and management of the sustainable drainage scheme shall be submitted for approval in writing by the City Council, as Local Planning Authority.

For the avoidance of doubt this shall include:

- Verification reporting providing photographic evidence of construction;
- Management and maintenance plan for the lifetime of the development which shall include the arrangements adoption by any public body or statutory undertaker, or any other arrangements to secure the operation of the sustainable drainage scheme throughout its lifetime.

The management and maintenance plan shall be implemented upon first use of the development and thereafter retained and maintained.

Reason – To manage flooding and pollution and to ensure that a managing body is in place for the sustainable drainage system and there is funding and maintenance mechanism for the lifetime of the development pursuant to policies SP1, EN14 and DM1 of the Manchester Core Strategy (2012).

12) (a) Notwithstanding drawings 1090 Rev P01 and 1095 Rev P01 stamped as received by the City Council, as Local Planning Authority, on the 1 October 2019, prior to the first use of this development, details of hard and soft landscaping treatments (including tree planting, street tree planting, boundary treatments and

appropriate samples of materials of hard landscaping) shall be submitted to and approved in writing by the City Council as local planning authority.

(b) The approved scheme shall be implemented prior to the first use of the development. If within a period of 5 years from the date of the planting of any tree or shrub, that tree or shrub or any tree or shrub planted in replacement for it, is removed, uprooted or destroyed or dies, or becomes, in the opinion of the local planning authority, seriously damaged or defective, another tree or shrub of the same species and size as that originally planted shall be planted at the same place.

Reason - To ensure that a satisfactory landscaping scheme for the development is carried out that respects the character and visual amenities of the area, in accordance with policies SP1, EN9 and DM1 of the Core Strategy.

13) Prior to the first use of the development hereby approved, full details of the specification and locations of bat and bird boxes, shall be submitted for approval in writing by the City Council as Local Planning Authority. The bat and bird boxes shall be installed prior to the first use of the development and therefore be retained and remain in situ.

Reason - To ensure the creation of new habitats and biodiversity in order to comply with policy EN15 of the Manchester Core Strategy (2012).

14) The development hereby approved shall be carried out in accordance with the Environmental Standards statement stamped as received by the City Council, as Local Planning Authority, on the 1 October 2019. A post construction review certificate/statement shall be submitted for approval, within a timescale that has been previously agreed in writing, to the City Council as Local Planning Authority.

Reason - In order to minimise the environmental impact of the development pursuant to policies SP1, T1-T3, EN4-EN7 and DM1 of the Core Strategy and the principles contained within The Guide to Development in Manchester SPD (2007) and the National Planning Policy Framework.

15) The development hereby approved shall achieve a post-construction Building Research Establishment Environmental Assessment Method (BREEAM) rating of at least an 'Excellent' rating. A post construction review certificate/statement shall be submitted for approval, within a timescale that has been previously agreed in writing, to the City Council as Local Planning Authority.

Reason - In order to minimise the environmental impact of the development, pursuant to policies SP1, T1-T3, EN4-EN7 and DM1 of the Core Strategy, and the principles contained within The Guide to Development in Manchester SPD (2007), and the National Planning Policy Framework.

16) Notwithstanding the acoustic report stamped as received by the City Council, as Local Planning Authority, on the 1 October 2019 and Revised acoustic assessment Rev 03 stamped as received by the City Council, as Local Planning Authority on the 28 November 2019 (a) Prior to the first use of the development hereby approved, details of any externally mounted ancillary plant, equipment and servicing shall be

submitted for approval. For the avoidance of doubt, externally mounted plant, equipment and servicing shall be selected and/or acoustically treated in accordance with a scheme designed so as to achieve a rating level of 5 db (L_{aeq}) below the typical background (L_{a90}) level at the nearest noise sensitive location.

(b) The approved scheme shall be implemented and prior to the first use of the development, a verification report will be required to validate that the work undertaken conforms to the recommendations and requirements approved as part of part (a) of this planning condition. The verification report shall include post completion testing to confirm the noise criteria has been met. In instances of non-conformity, these shall be detailed along with mitigation measures required to ensure compliance with the noise criteria. Any mitigation measures shall be implemented in accordance with a timescale to be agreed with the City Council, as Local Planning Authority, and thereafter retained and maintained in situ.

Reason - To minimise the impact of plant on the occupants of the development pursuant to policies SP1 and DM1 of the Manchester Core Strategy (2012) and saved policy DC26 of the Unitary Development Plan for the City of Manchester (1995).

17) Notwithstanding the acoustic report stamped as received by the City Council, as Local Planning Authority, on the 1 October 2019 and Revised acoustic assessment Rev 03 stamped as received by the City Council, as Local Planning Authority on the 28 November 2019 (a) Prior to the first use of the development hereby approved, details of a scheme for acoustic treatment of the premises shall be submitted for approval in writing by the City Council, as Local Planning Authority. The approved scheme shall then be implemented prior to first use and thereafter retained and maintained.

Where entertainment noise is proposed the L_{Aeq} (entertainment noise) shall be controlled to 10 dB below the L_{A90} (without entertainment noise) in each octave band at the façade of the nearest noise sensitive location.

(b) The approved scheme shall be implemented and prior to the first use of the development, a verification report will be required to validate that the work undertaken conforms to the recommendations and requirements approved as part of part (a) of this planning condition. The verification report shall include post completion testing to confirm the noise criteria has been met. In instances of non-conformity, these shall be detailed along with mitigation measures required to ensure compliance with the noise criteria. Any mitigation measures shall be implemented in accordance with a timescale to be agreed with the City Council, as Local Planning Authority, and thereafter retained and maintained in situ.

Reason - To ensure no unacceptable noise outbreak from the development pursuant to policies SP1 and DM1 of the Manchester Core Strategy (2012) and saved policy DC26 of the Unitary Development Plan for the City of Manchester (1995).

18) The development hereby approved shall be carried out in accordance with site waste management strategy and drawing 1095 Rev P01 stamped as received by the City Council, as Local Planning Authority, on the 1 October 2019. The details shall

be implemented prior to the first use of the development and thereafter retained and maintained in situ.

Reason - To ensure adequate refuse arrangements are put in place for the development pursuant to policies EN19 and DM1 of the Manchester Core Strategy.

19) Prior to the first use of the development, details of a scheme to extract fumes, vapours and odours from the premises shall be submitted for approval in writing by the City Council, as Local Planning Authority. The approved scheme shall then be implemented prior to the first use of the development and thereafter retained and maintained in situ.

Reason – To ensure appropriate fume extraction is provided for the development pursuant to policies SP1 and DM1 of the Manchester Core Strategy and saved policy DC10 of the Unitary Development Plan for the City of Manchester (1995).

20) The development hereby approved shall include a building and site lighting scheme and a scheme for the illumination of external areas during the period between dusk and dawn. Full details of such a scheme shall be submitted for approval in writing by the City Council, as Local Planning Authority before the first use of the development hereby approved. The approved scheme shall be implemented in full prior to the first use of development and shall remain in operation for so long as the development is occupied.

Reason - In the interests of amenity, crime reduction and the personal safety of those using the proposed development in order to comply with the requirements of policies SP1 and DM1 of the Core Strategy.

21) If any lighting at the development hereby approved, when illuminated, causes glare or light spillage which in the opinion of the Council as local planning authority causes detriment to adjoining and nearby residential properties, within 14 days of a written request, a scheme for the elimination of such glare or light spillage shall be submitted to the Council as local planning authority and once approved shall thereafter be retained in accordance with details which have received prior written approval of the City Council as Local Planning Authority.

Reason - In order to minimise the impact of the illumination of the lights on the occupiers of nearby residential accommodation, pursuant to policies SP1 and DM1 of the Core Strategy.

22) The development shall be carried out in accordance with the Crime Impact Statement prepared by Design for Security at Greater Manchester Police stamped as received by the City Council, as Local Planning Authority, on the 1 October 2019. The development shall only be carried out in accordance with these approved details. The development hereby approved shall not be occupied or used until the Council as local planning authority has acknowledged in writing that it has received written confirmation of a Secured by Design accreditation.

Reason - To reduce the risk of crime pursuant to policies SP1 and DM1 of the Core Strategy and to reflect the guidance contained in the National Planning Policy Framework.

23) Prior to the first use of the development hereby approved, a travel plan framework shall be submitted for approval in writing by the City Council, as Local Planning Authority.

In this condition a Travel Plan means a document which includes:

- i) the measures proposed to be taken to reduce dependency on the private car by those studying and working at the development;
- ii) a commitment to surveying the travel patterns of students and staff during the first three months of the first use of the building and thereafter from time to time
- iii) mechanisms for the implementation of the measures to reduce dependency on the private car
- iv) measures for the delivery of specified travel plan services
- v) measures to monitor and review the effectiveness of the Travel Plan in achieving the objective of reducing dependency on the private car

Within six months of the first use of the building, a Travel Plan which takes into account the information about travel patterns gathered pursuant to item (ii) above shall be submitted for approval in writing by the City Council as Local Planning Authority. Any Travel Plan which has been approved by the City Council as Local Planning Authority shall be implemented in full at all times when the development hereby approved is in use.

Reason - To assist promoting the use of sustainable forms of travel for students, staff and visitors, pursuant to policies T1, T2 and DM1 of the Manchester Core Strategy (2012).

24) (a) Prior to the first use of the building, the provision of 112 cycle spaces, as indicated on drawing 1095 Rev P01 stamped as received by the City Council, as Local Planning Authority, on the 1 October 2019 shall be implemented and retained and maintained in situ for as long as the development remains in use.

(b) Within 12 months of the first use of the development hereby approved a cycle parking review shall be submitted for approval in writing by the City Council, as Local Planning Authority. The review shall detail uptake of cycle parking facilities at the development. Where the review shows that there is demand for additional cycle parking at the development details shall be submitted and a timescale for implementation shall be provided. The additional cycle parking shall then be implemented in accordance with the agreed timescale.

Reason – To ensure there is sufficient cycles stand provision at the development and the occupants in order to support modal shift measures pursuant to policies SP1, T1, T2 and DM1 of the Manchester Core Strategy (2012).

25) Prior to the first use of the development hereby approved, the six disabled car parking spaces, as indicated on drawing 1095 Rev P01 stamped as received by the City Council, as Local Planning Authority, on the 1 October 2019 shall be laid out, demarcated and made available. This shall include the installation of the layby which can also be used for a further two disabled car parking spaces as required.

The car parking layout shall be retained and maintained for as long as the development remains in use.

Reason – To ensure disabled car parking is available for the development pursuant to policies SP1, T1, and DM1 of the Manchester Core Strategy (2012).

26) Prior to the first use of the development hereby approved, a scheme of highway works and details of footpaths reinstatement/public realm shall be submitted for approval in writing by the City Council, as Local Planning Authority.

For the avoidance of doubt this shall include the following:

- Footway improvement and reinstatement works around the perimeter of the application site including provision of street trees. These footway improvements shall include details of materials, including natural stone or other high quality materials to be used for the footpaths and for the areas between the pavement and the line of the proposed building/public realm;
- Alternation/re-instatement works to the highway to create new access off Great Ducie Street;
- Provision of a pedestrian improvements to the junction of New Bridge Street/Trinity Way.

The approved scheme shall be implemented and be in place prior to the first use of the development hereby approved and thereafter retained and maintained in situ.

Reason - To ensure safe access to the development site in the interest of pedestrian and highway safety pursuant to policies SP1, EN1 and DM1 of the Manchester Core Strategy (2012).

27) Prior to the first use of the use of the development 6 electric car charging points shall be installed within the car parking area (as indicated on drawing 1095 Rev P01 stamped as received by the City Council, as Local Planning Authority, on the 1 October 2019) in accordance with a scheme submitted for approval in writing by the City Council, as Local Planning Authority. The approved scheme shall be implemented prior to the first use of the development and made available and thereafter retained for as long as the development is in place.

Reason – In the interest of promoting alternatives and minimising the impact of the development on air quality pursuant to policies EN16 of the Manchester Core Strategy (2012).

28) Notwithstanding the TV reception survey stamped as received by the City Council, as Local Planning Authority, on the 1 October 2019, within one month of the practical completion or at any other time during the construction of the development if requested in writing by the City Council as local planning authority in response to identified television signal reception problems within the potential impact area a study shall identify such measures necessary to maintain at least the pre-existing level and quality of signal reception identified in the survey carried out above. The measures identified must be carried out either before the building is first used or

within one month of the study being submitted to the City Council as local planning authority, whichever is the earlier.

Reason - To provide an indication of the area of television signal reception likely to be affected by the development to provide a basis on which to assess the extent to which the development during construction and once built, will affect television reception and to ensure that the development at least maintains the existing level and quality of television signal reception - In the interest of residential amenity, as specified in policy DM1 of Core Strategy.

29) Prior to the first use of the development hereby approved a signage strategy for the entire building shall be submitted for approval in writing by the City Council, as Local Planning Authority. The approved strategy shall then be implemented and used to inform any future advertisement applications for the building.

Reason – In the interest of visual amenity pursuant to policies SP1 and DM1 of the Manchester Core Strategy (2012).

30) Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (or any order revoking and re-enacting that Order with or without modification) the premises shall only be used for educational purposes (Use Class D1), and for the ancillary use of its facilities by the public, and for no other purpose.

Reason – In the interest of retaining the educational use pursuant to policies EC1, EC4 of the Manchester Core Strategy (2012).

31) Prior to the first use of the development hereby approved, details of a servicing and operational management plan shall be submitted for approval in writing by the City Council, as Local Planning Authority. For the avoidance of doubt this shall include how servicing arrangements will be managed at the development including ensuring the access road remains unrestricted. The approved plan shall be implemented upon the first use of the development and thereafter retained and maintained.

Reason – In the interest of highway and pedestrian safety pursuant to policies SP1, T2 and DM1 of the Manchester Core Strategy (2012).

32) When the development is first occupied, deliveries, servicing and collections including waste collections shall not take place outside the following hours:

Monday to Saturday 07:30 to 20:00
No deliveries on Sundays

Reason – In the interest of residential amenity pursuant to policies SP1 and DM1 of the Manchester Core Strategy (2012).

33) The roof terrace and external social areas as shown on drawings 2296 Rev P01 and 1095 Rev P01 stamped as received by the City Council, as Local Planning Authority, on the 1 October 2019 shall not be open outside the following hours:-

Monday to Sunday 08:00 to 23:00

There shall be no amplified music or sound on the roof terrace at any time.

Reason - In interests of amenity in order to reduce noise and general disturbance in accordance with saved policy DC26 of the Unitary Development Plan for the City of Manchester and policies SP1 and DM1 of the Core Strategy.

34) The development hereby approved shall include for full disabled access to be provided to all areas of public realm and via the main entrances and to the floors above.

Reason - To ensure that satisfactory disabled access is provided by reference to the provisions Core Strategy policy DM1.

35) All windows at ground level, unless shown otherwise on the approved drawings detailed in condition 2, shall be retained as a clear glazed window opening at all times and views into the premises shall not be screened or obscured in any way.

Reason - The clear glazed window(s) is an integral and important element in design of the ground level elevations and are important in maintaining a visually interesting street-scene consistent with the use of such areas by members of the public, and so as to be consistent with saved policy DC14 of the Unitary Development Plan for the City of Manchester and policies SP1 and DM1 of the Core Strategy.

36) Prior to the first use of the development, details of any roller shutters to the ground floor of the building shall be submitted for approval in writing by the City Council, as Local Planning Authority. For the avoidance of doubt the shutters shall be fitted internally to the premises. The approved details shall be implemented prior to the first use of the building and thereafter retained and maintained in situ.

Reason – To ensure that the roller shutters are appropriate in visual amenity terms pursuant to policies SP1, EN1 and DM1 of the Manchester Core Strategy (2012).

37) Prior to the first occupation of the development, details of the siting, scale and appearance (including samples of materials) of the permanent boundary treatments shall be submitted for approval in writing by the City Council, as Local Planning Authority. The approved scheme shall then be implemented and be in place prior to the first use of the development. The boundary treatment shall be retained and maintained in situ thereafter and notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (or any order revoking or re-enacting that Order with or without modification) no boundary treatment shall be erected on site, other than that shown on the approved plans.

Reason – To ensure that appropriate boundary treatment is put in place in the interest of visual amenity pursuant to policies SP1 and DM1 of the Manchester Core Strategy (2012).

38) (a) Prior to the first use of the development hereby approved, details of the interim boundary treatment arrangements for the north and eastern boundaries of

the site shall be submitted for approval in writing by the City Council, as Local Planning Authority. For the avoidance of doubt this shall include the siting, scale and appearance of the boundary treatment together with details of the implementation of the boundary treatment, duration of the interim period and details to remove the boundary treatment once the surrounding development plots have been developed/interim period has expired.

(b) The approved boundary treatment shall be implemented prior to the first use of the development and retained and maintained for the agreed interim period.

(c) Once the interim period has expired the boundary treatment shall be removed within one month of expiration.

Reason – To ensure there is adequate boundary treatment to the surrounding development plots prior to them being development pursuant to policies SP1 and DM1 of the Manchester Core Strategy (2012).

39) Prior to the first use of the development hereby approved, details of the siting, scale and appearance of the solar panels to the roof (including cross sections). The approved details shall then be implemented prior to the first use of the development and thereafter retained and maintained in situ.

Reason – In the interest of ensuring the solar panels are installed and to ensure that they are appropriate in terms of visual amenity pursuant to policies SP1, EN1, EN6 and DM1 of the Manchester Core Strategy (2012).

40) Prior to the first use of the development hereby approved, details of an operational and event management plan for the use of the theatre and other spaces within the building for public beyond the D1 use. For the avoidance of doubt, this shall include details of operating hours, travel planning (including supporting those with disabilities), servicing arrangements for events and dispersal of visitors from the building. The approved operational and event management plan shall be implemented upon first use of the development and thereafter the use shall remain operational in accordance with this document.

Reason – To ensure that ancillary uses outside the main college use operate without having an unacceptable impacts on surrounding residential amenity together with encouraging sustainable travel by visitors pursuant to policies SP1, T2 and DM1 of the Manchester Core Strategy (2012) and saved policy DC26 of the Unitary Development Plan for the City of Manchester (1995).

Informatives

- Any signage, wayfinding, banners or any other advertisements to be installed in and around the application site for the purpose of the promotion of the developments and routes to it may require consent under the Town and Country Planning (Control of Advertisements) (England) Regulations 2007.

Local Government (Access to Information) Act 1985

The documents referred to in the course of this report are either contained in the file(s) relating to application ref: 124995/FO/2019 held by planning or are City Council planning policies, the Unitary Development Plan for the City of Manchester, national planning guidance documents, or relevant decisions on other applications or appeals, copies of which are held by the Planning Division.

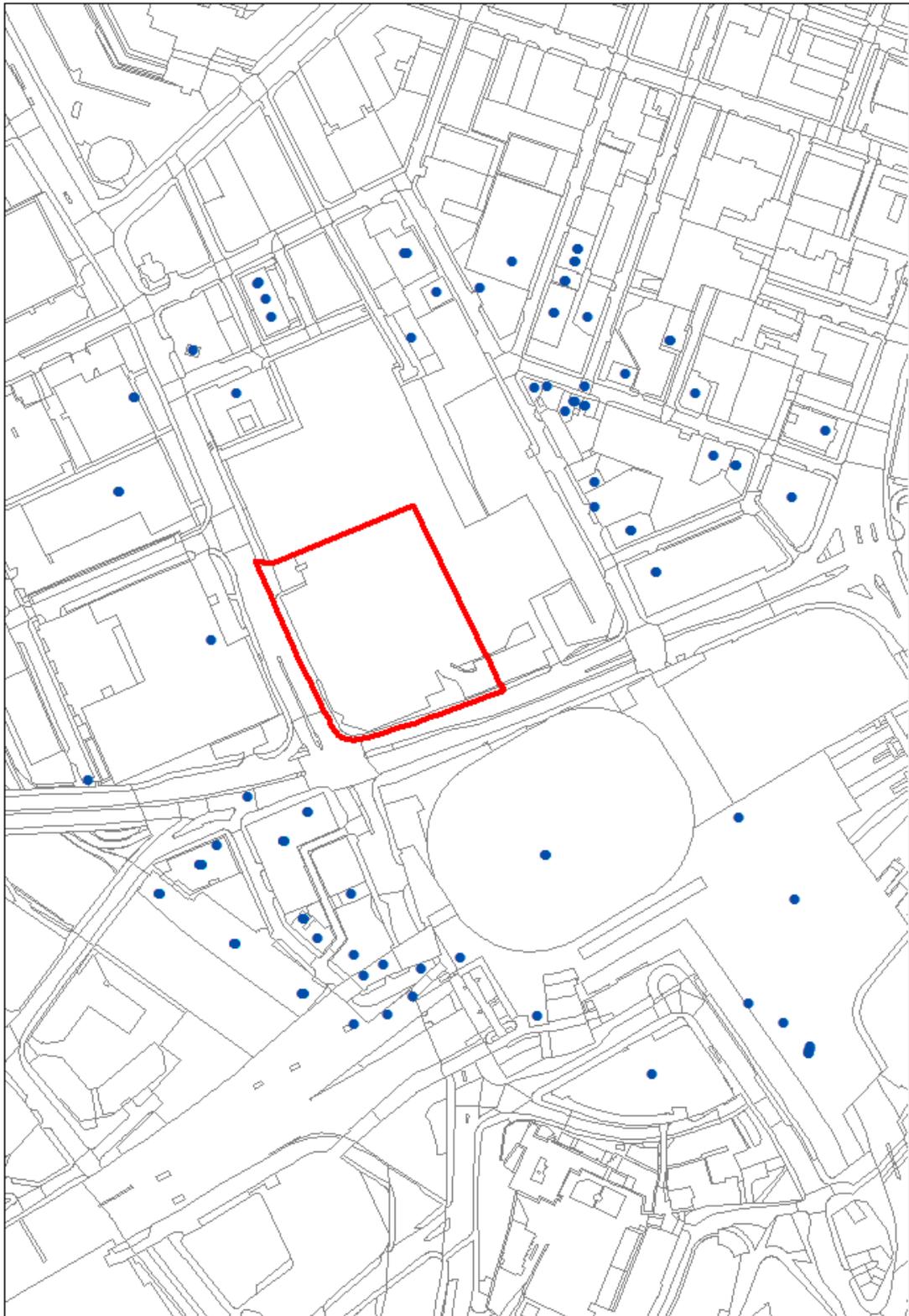
The following residents, businesses and other third parties in the area were consulted/notified on the application:

**Highway Services
Environmental Health
Neighbourhood Team Leader (Arboriculture)
Strategic Development Team
MCC Flood Risk Management
Work & Skills Team
Greater Manchester Police
Historic England (North West)
Environment Agency
Transport For Greater Manchester
Greater Manchester Archaeological Advisory Service
United Utilities Water PLC
Greater Manchester Ecology Unit
Planning Casework Unit
Salford City Council**

A map showing the neighbours notified of the application is attached at the end of the report.

Representations were received from the following third parties:

Relevant Contact Officer : Jennifer Atkinson
Telephone number : 0161 234 4517
Email : j.atkinson@manchester.gov.uk



 Application site boundary  Neighbour notification
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